



WELCOME

Delaware Ave Reconstruction &
Butler Ave Trail and Sidewalk Improvements
OPEN HOUSE

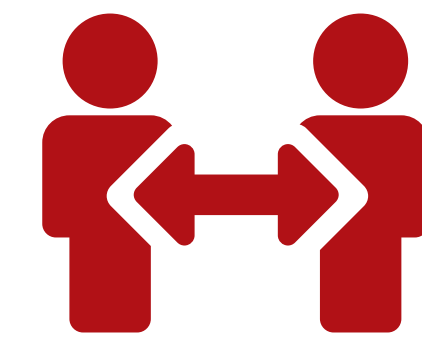
Project Background

Dakota County and the cities of Mendota Heights and West St. Paul are planning to reconstruct Delaware Avenue from Marie Avenue to Dodd Road and make improvements on Butler Avenue from Delaware Avenue to Manomin Avenue. The project will add curb and gutter, multi-use trail, and improve drainage and utilities on Delaware Avenue. The project will also add a multi-use trail and sidewalk on Butler Avenue.



Project Area Map

PROJECT GOALS:



ENGAGE THE PUBLIC

to ensure active participation and understanding for the project



INCORPORATE PUBLIC INPUT

in evaluating and identifying recommended improvements



IMPROVE PEDESTRIAN AND BIKE SAFETY

and access to area schools and parks



URBANIZE THE ROADWAY

by adding curb and gutter, improving roadway maintenance, and improving stormwater management



MITIGATE PROPERTY IMPACTS

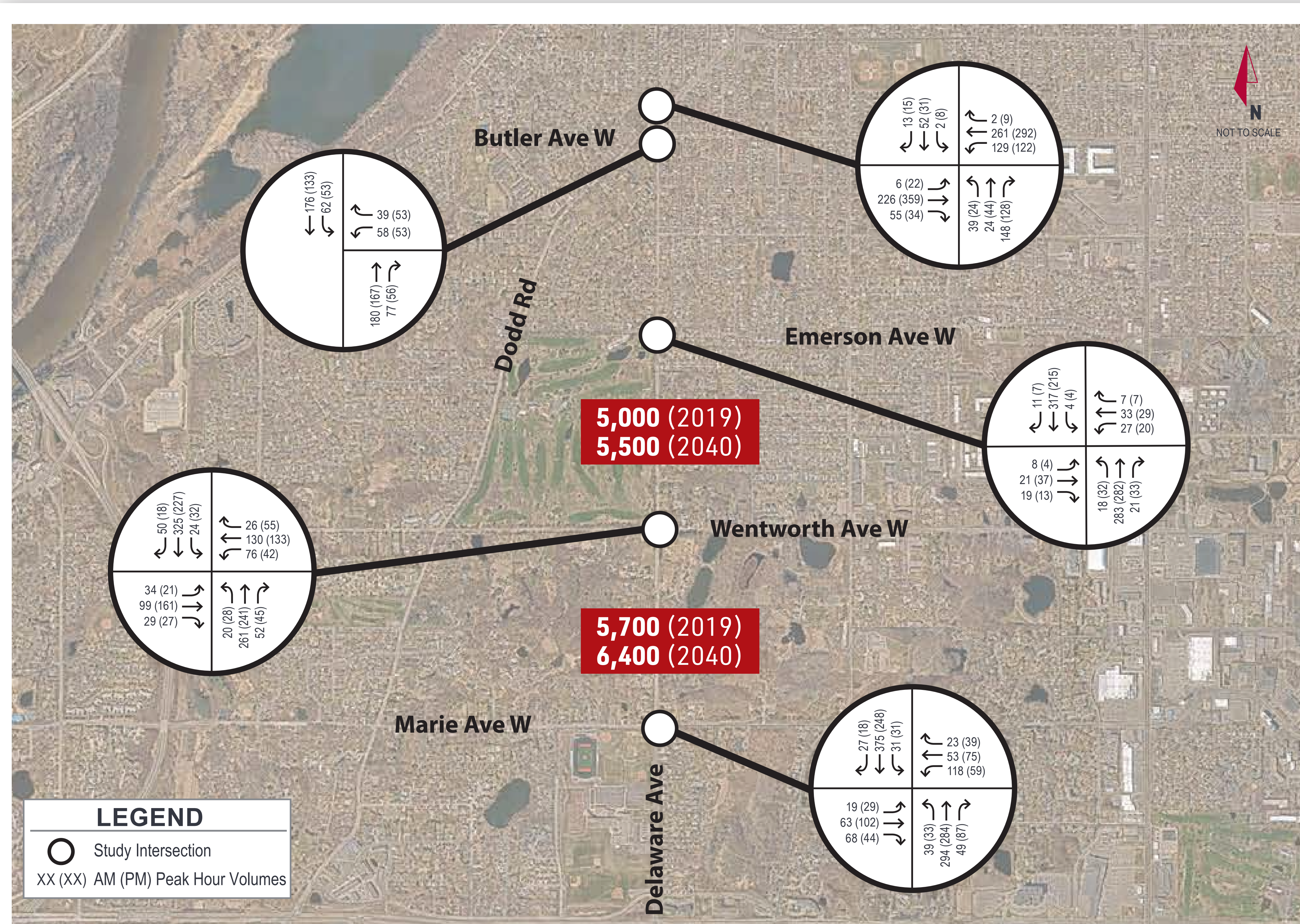
Evaluate design options to minimize property impacts while achieving other project goals



EVALUATE INTERSECTION AND ROADWAY SAFETY

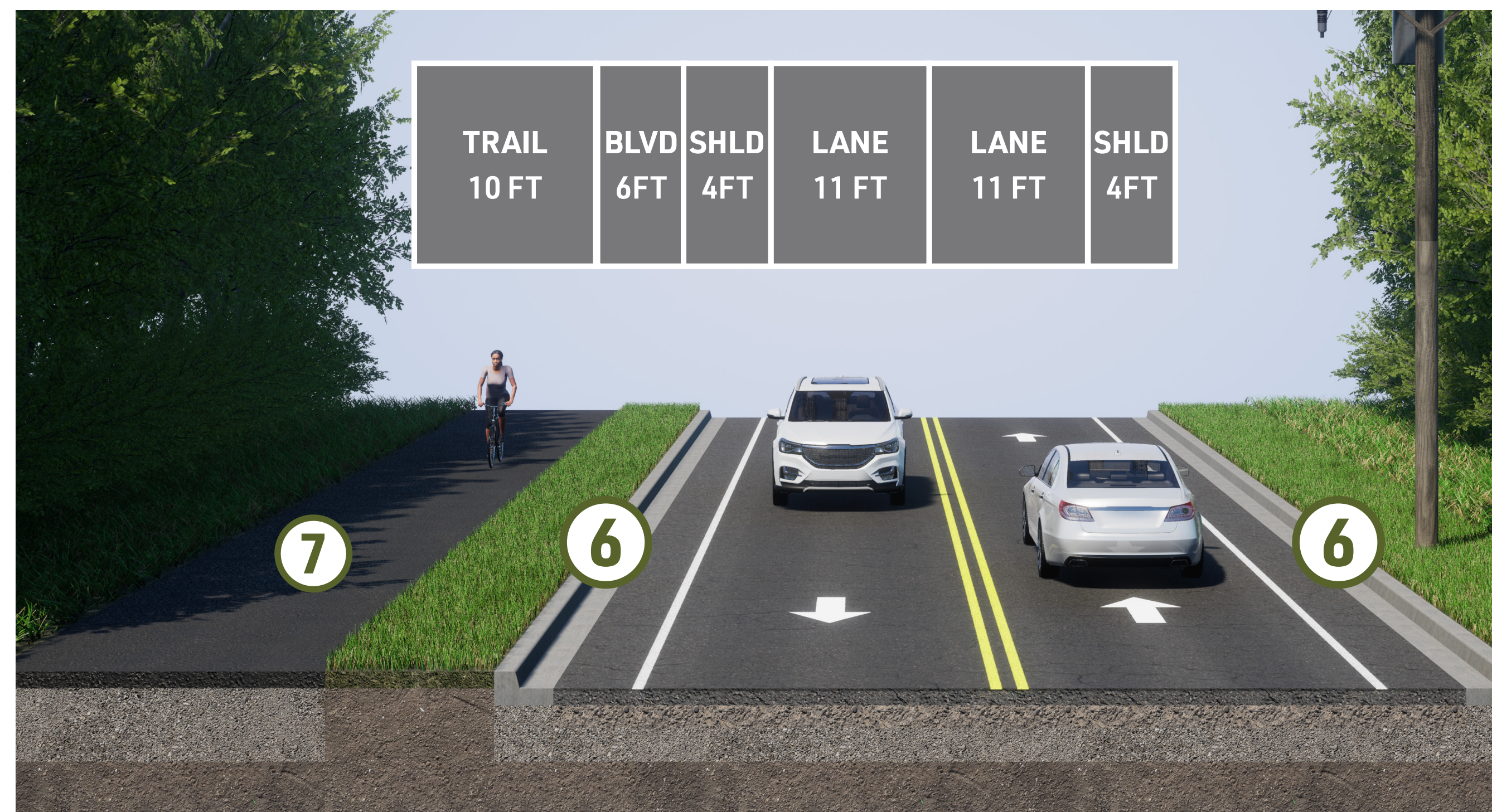
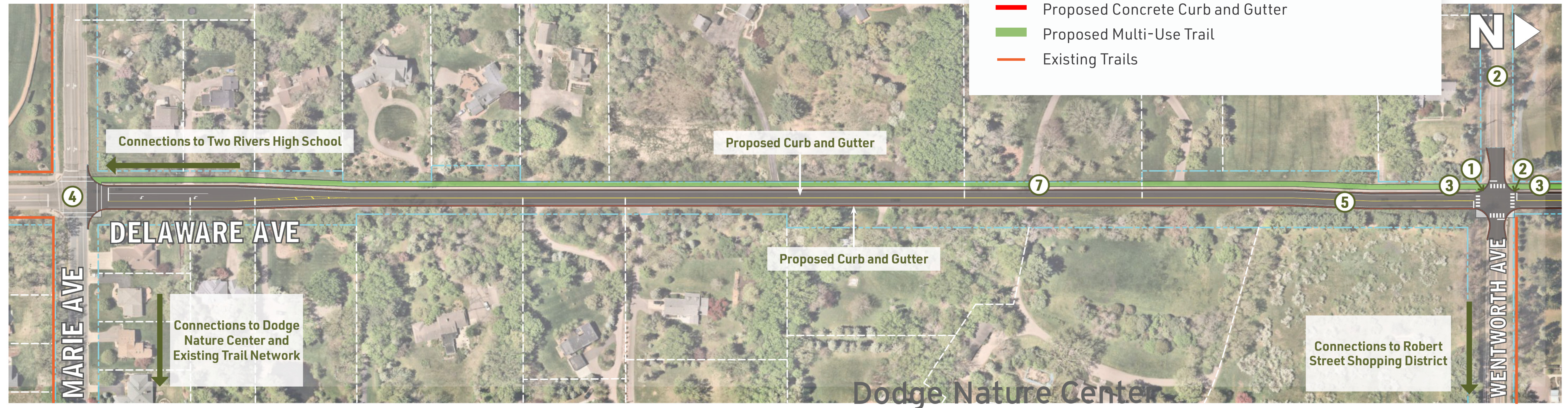
by evaluating turn lane needs, intersection control, and other safety improvements

Existing Traffic Counts and Daily Traffic



EXISTING YEAR 2024
PEAK HOUR AND AVERAGE DAILY TOTAL TRAFFIC VOLUMES

Segment 1: Marie Ave to Wentworth Ave

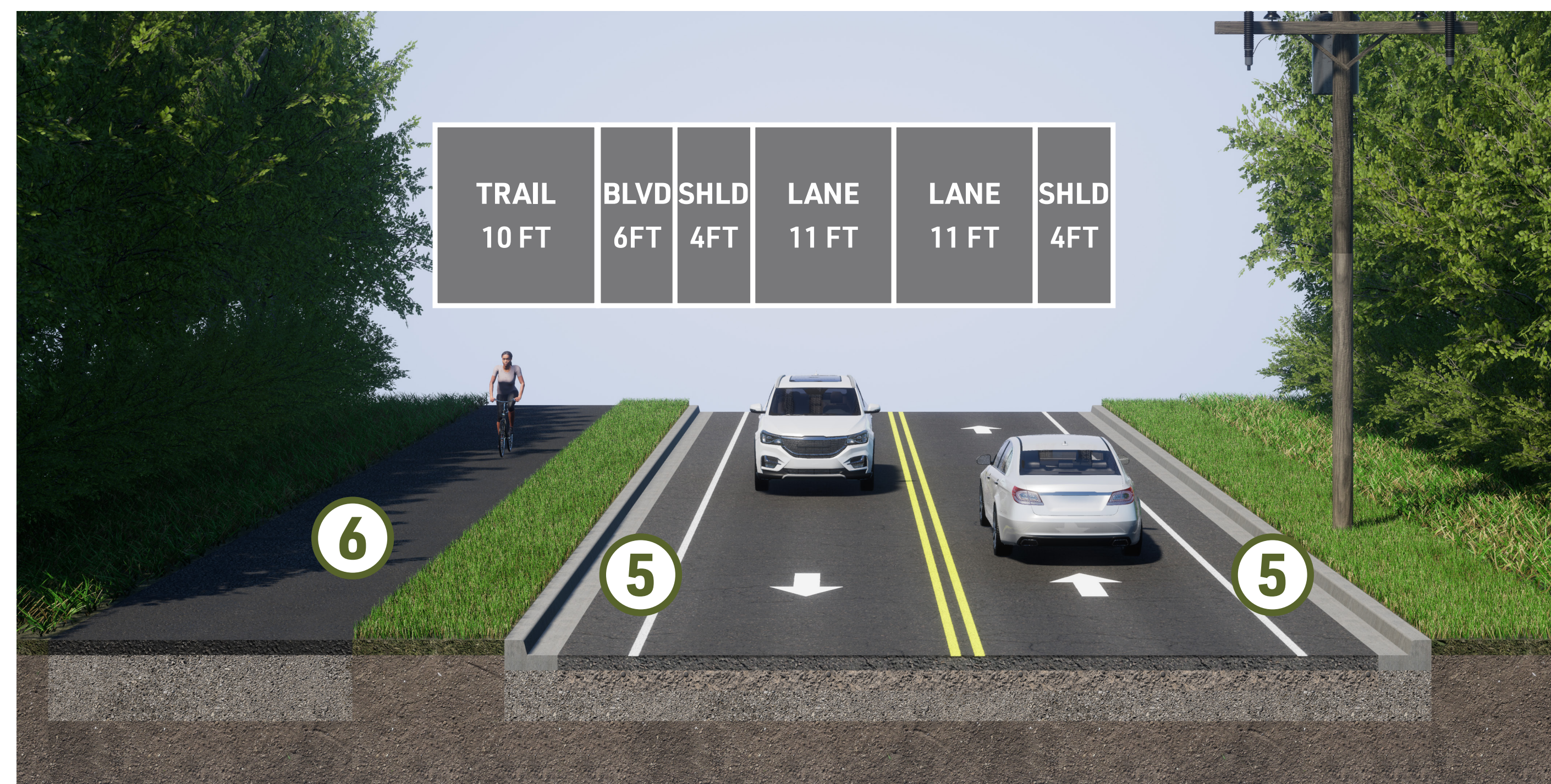
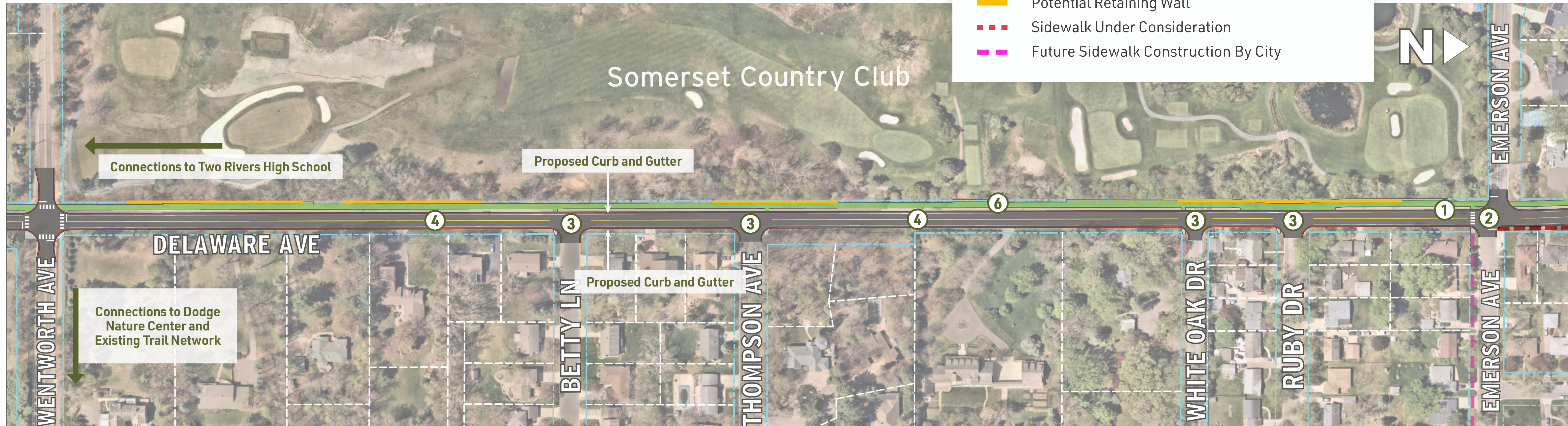


Proposed Delaware Avenue Typical Section (Looking North)
Segment 1: Marie Ave to Wentworth Ave

Segment Considerations:

- ① Curb bumpouts to reduce pedestrian crossings and promote slowing of vehicles
- ② Install additional signage and pavement markings for improved advance stop sign warning. Consider increased lighting for improved nighttime sight distance at intersection.
- ③ Improved intersection sight distance
- ④ Maintain existing intersection lane configuration at Marie Avenue
- ⑤ Roadway alignment shift at Wentworth Avenue to reduce impacts to Somerset Country Club
- ⑥ On-street parking not permitted
- ⑦ Proposed multi-use trail on one side of Delaware Avenue only

Segment 2: **Wentworth Ave to Emerson Ave**



Proposed Delaware Avenue Typical Section (Looking North)
Segment 2: **Wentworth Ave to Emerson Ave**

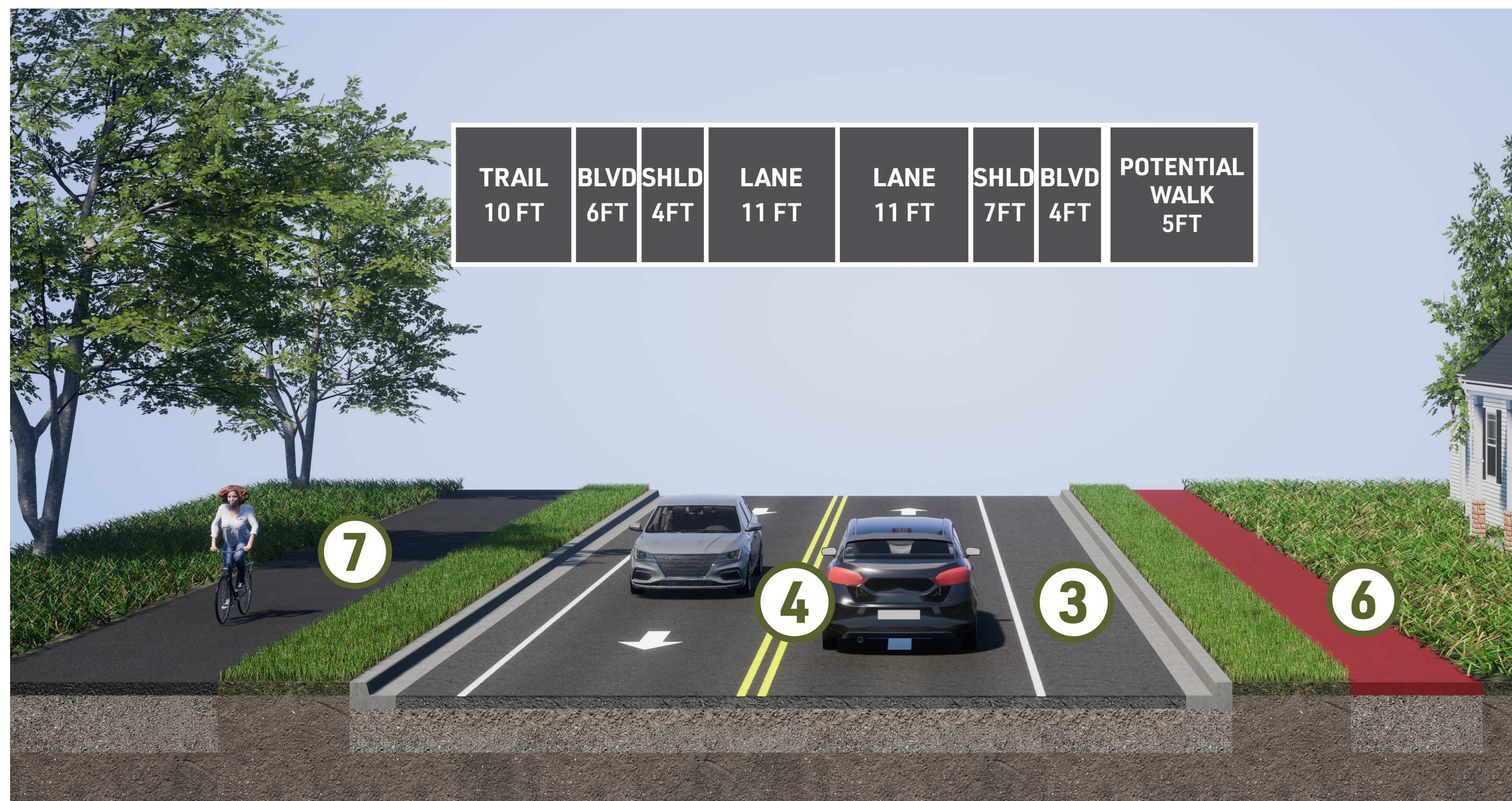
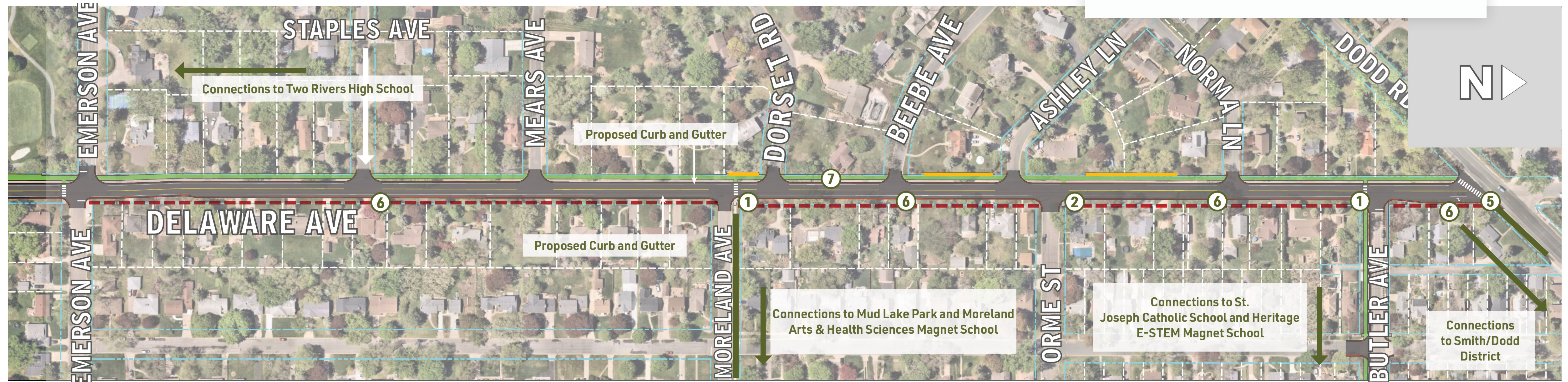
Segment Considerations:

- ① Relocate city lift station generator and clear vegetation for improved intersection sight distance
- ② Dedicated pedestrian crossing with advance warning signage and markings
- ③ Accessible crossing location
- ④ Roadway alignment shift at Wentworth Avenue to reduce impacts to Somerset Country Club
- ⑤ On-street parking not permitted
- ⑥ Proposed multi-use trail on one side of Delaware Avenue only

Segment 3: Emerson Ave to Dodd Road

Legend:

- Proposed Roadway
- Proposed Concrete Curb and Gutter
- Proposed Multi-Use Trail
- Potential Retaining Wall
- Sidewalk Under Consideration



Proposed Delaware Avenue Typical Section (Looking North)
Segment 3: Emerson Ave to Dodd Road

Segment Considerations:

- Dedicated pedestrian crossing with advance warning signage and markings
- Accessible crossing location
- On-street parking allowed
- Approximately maintain existing roadway centerline
- Addition of a new, northbound right turn lane
- Consideration for sidewalk along East side of Delaware Avenue (see notes to right)
- Proposed multi-use trail on one side of Delaware Avenue only

Share your feedback! We want to know how important an additional off-street pedestrian facility is to residents along the east side of Delaware Avenue.



East Side Sidewalk Considerations:

- Offers safe, off-street access to crossings and the proposed pedestrian trail along the west side of Delaware Avenue
- May require loss of on-street parking on East side of road
- Requires additional tree clearing and increased impacts to private property (retaining walls, fencing, landscaping, etc.)
- Requires temporary easements from impacted private properties
- Requires snow clearing responsibilities by adjacent residents

Butler Avenue Segment

Legend:

- Proposed Roadway
- Proposed Concrete Curb and Gutter
- Proposed Multi-Use Trail
- Sidewalk Under Consideration
- Proposed Concrete Sidewalk



Proposed Butler Avenue Typical Section (Looking East)

Project Background:

- High priority pedestrian and bicycle gap in Dakota County 2040 Transportation Plan
- Improvements recommended in County School Travel Safety Assessment completed in 2021 for adjacent schools
- Provide continuous connections to newly constructed crossing improvements at Seminole Avenue and area schools
- Add trail and sidewalk facilities while minimizing impacts to adjacent property owners (requires roadway typical section changes)



Trail Side Evaluation Matrix

 Preferred

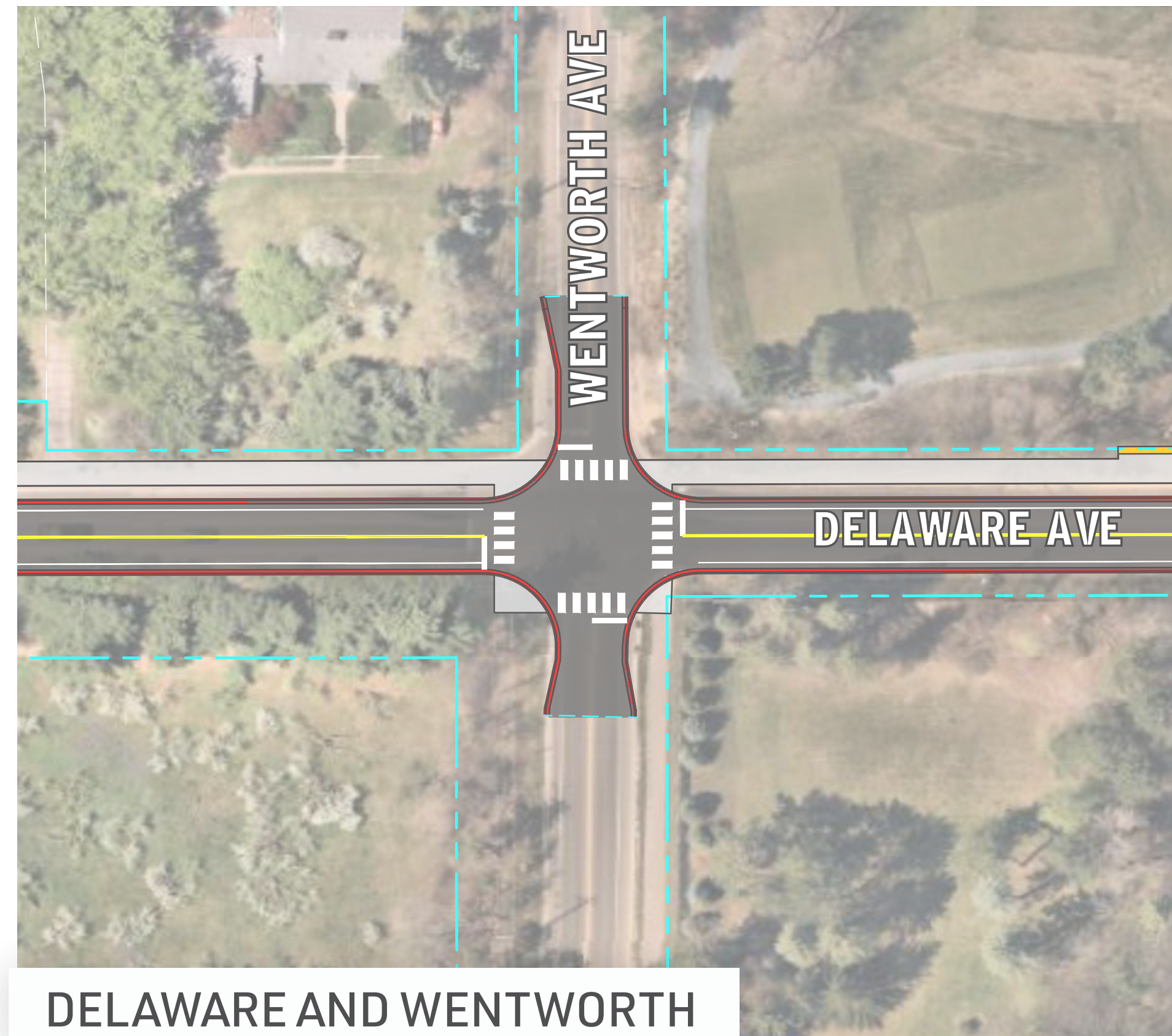
Evaluation Criteria	SEGMENT 1: MARIE AVE TO WENTWORTH AVE		SEGMENT 2: WENTWORTH AVE TO EMERSON AVE		SEGMENT 3: EMERSON AVE TO DODD RD	
	Construct Trail on West Side (Mendota Heights)	Construct Trail on East Side (West St. Paul)	Construct Trail on West Side (Mendota Heights)	Construct Trail on East Side (West St. Paul)	Construct Trail on West Side (Mendota Heights)	Construct Trail on East Side (West St. Paul)
# of Impacted Driveways	11 DWs	8 DWs	0 DWs	10 DWs	16 DWs	27 DWs
Pedestrian and Bicycle Safety/ Regional Connectivity Considerations	Provides most direct connection to Two Rivers High School	Requires crossing at Marie Avenue to access High School	<ul style="list-style-type: none">Provides connection to High SchoolNo side street crossings for pedestrians	<ul style="list-style-type: none">Requires crossing at Wentworth Avenue or Emerson Avenue for access to Two Rivers High SchoolProvides direct connection to new trail from WSP side streets	<ul style="list-style-type: none">Provides connection to High SchoolMid-Block or accessible crossings provided at WSP side streets	<ul style="list-style-type: none">Provides direct connection to new trail from ped/bike facilities in WSPCrossing at WentworthMid-Block or accessible crossings provided at MH side streetssResidents could walk along parkable shoulder
Temporary Easements	11 TEs	4 TEs	1 TE (Country Club)	14 TEs	25 TEs	36 TEs
Permanent Easements	6 PEs	3 PEs	1 PE (Country Club)	14 PEs	25 PEs	36 PEs
Required Retaining Walls	0	0	~3-4 RWs	1 RW	~ 3 RWs	~9-10 RWs
Driveway Parking	No driveways will lose the ability to park a car	No driveways will lose the ability to park a car	No driveways will lose the ability to park a car	3 Driveways will lose ability to park a car	No driveways will lose the ability to park a car	No driveways will lose the ability to park a car
Impacted Driveways with significant reconstruction	6 DWs	2 DWs	0	1 DW	0	7 DWs
Private Property Feature Impacts (Driveway Monument, Driveway Retaining Wall, etc.)	1 property with driveway entrance monument	2 properties with driveway entrance monuments	Impacts to golf course	Grading and driveway reconstruction within 5-feet of homes	<ul style="list-style-type: none">Impacts 2 decorative drivewaysImpacts 1 existing retaining wallImpacts 1 existing fenceImpacts to additional landscaping areas	<ul style="list-style-type: none">Impacts at City Lift Station SiteImpacts one decorative dirvewayImpacts 7 existing retaining wallsImpacts 5 existing fenceImpacts 7 existing staircasesImpacts to additional landscaping areas
Tree and Vegetation Impacts	~ 25 Trees and 0.5 Acres	~ 34 Trees and 0.35 Acres	~ 15 Trees & 1.5 Acres	~33 Trees & 0.35 Acres	~ 36 Trees & 0.2 Acres	~78 Trees
Private Impacts	Power poles and underground utilities on both sides of roadway	Power poles and underground utilities on both sides of roadway	Minimal private utilities	<ul style="list-style-type: none">Power poles on east side of roadwayGas main on east side of roadway where presentMost underground utilities on east side of roadway	<ul style="list-style-type: none">Dual gas mains on both sides of roadwayOther utilities on both sides of roadway	<ul style="list-style-type: none">Power poles on east side of roadway (where present)Dual gas mains on both sides of roadwayOther utilities on both sides of roadwayCity forcemain in boulevard
On-Street Parking	Not provided	Not provided	Not provided	Not provided	Not provided	On-Street parking provided on non-trail side of street

Intersection Considerations

Legend:

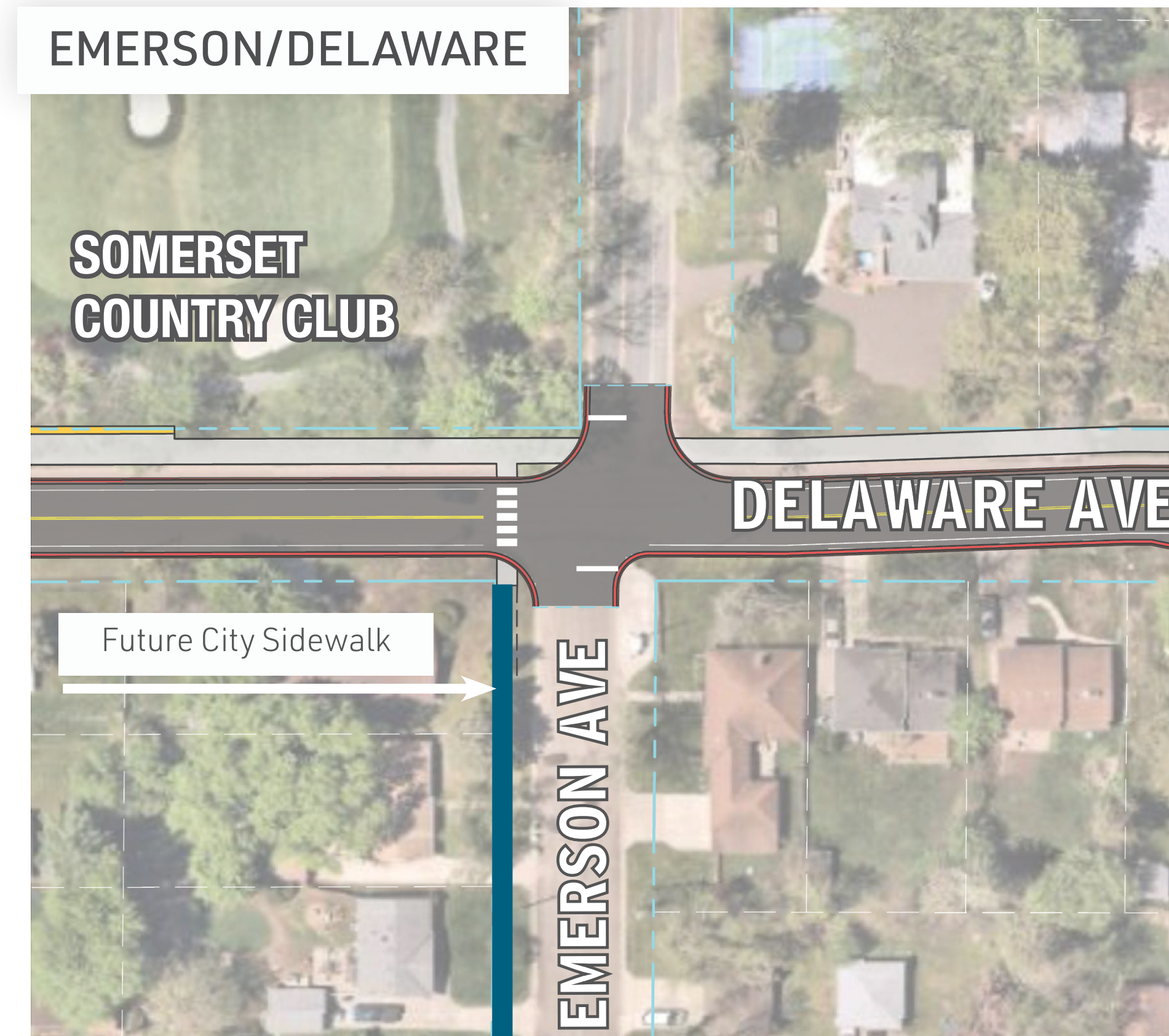
- Proposed Roadway
- Proposed Concrete Curb and Gutter
- Proposed Trails

- Potential Retaining Wall
- Future City Sidewalk



A roundabout was considered at this intersection and is not recommended due to the following:

- Significant increase in private property impacts vs. an all-way stop intersection
- Higher construction cost
- All-way stop intersection provides acceptable traffic operations
- No history of serious or fatal crashes at intersection
- Additional signage, markings, lighting, and geometric improvements are being considered to improve visibility and stop sign compliance at intersection



Intersection skew and sight distance were evaluated and the following is recommended:

- Intersection sight distance deficiency contributes to majority of crashes
- No realignment of Emerson Avenue is proposed
- Relocate existing generator and clear trees to improve intersection sight distance conditions
- Provide a pedestrian crossing from planned City sidewalk to new multi-use trail
- Stop signs on Delaware Avenue are not proposed. Additional advance stop signage being considered on Emerson Avenue



The project will add:

- A northbound right turn lane at Dodd Road
- A marked and signed crosswalk at the intersection of Delaware and Butler Avenues.



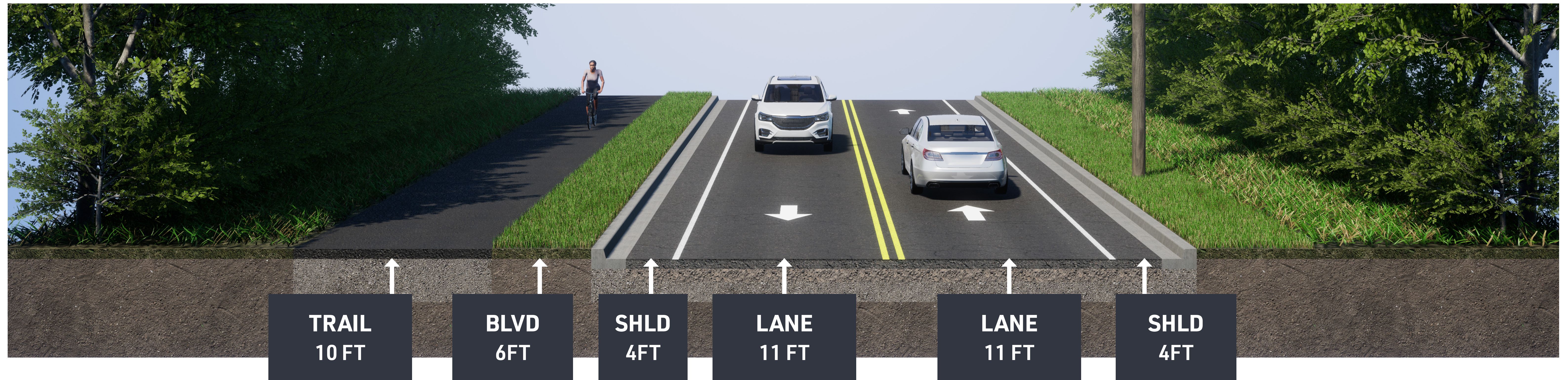
Current conditions at Delaware Ave and Emerson Ave

Example of a marked and signed pedestrian crossing nearby on Butler Ave and Seminole Ave



Delaware Avenue Roadway Typical Section

Proposed Roadway Typical Section (Segment 1: Marie Avenue to Wentworth Avenue and Segment 2: Wentworth Avenue to Emerson Avenue)



Proposed Roadway Typical Section (Segment 3: Emerson Avenue to Dodd Road)



<i>Open House #1</i>	
<i>Preliminary Design and Alternatives Analysis</i>	
<i>Open House #2</i>	
<i>Final Design</i>	
<i>Open House #3</i>	
<i>Right-of-Way Acquisition (as needed)</i>	
<i>Bidding</i>	
<i>Advance Construction Work (as feasible)</i>	
<i>Private Utility Relocation (anticipated)</i>	
<i>Construction</i>	



*Use this QR code to
check out our project website, take the project
survey, and comment on the project map*



Dakota County
www.co.dakota.mn.us